

DATE OF MEETING | February 1, 2021 |

AUTHORED BY | CALEB HORN, PLANNER, CURRENT PLANNING |

SUBJECT | **DEVELOPMENT PERMIT APPLICATION NO. DP1173 –  
1421 PRINCESS ROYAL AVENUE** |

## OVERVIEW

### **Purpose of Report**

To present for Council's consideration a development permit application for a four-storey multi-family residential building with 16 dwelling units and under-the-building parking. |

### **Recommendation**

That Council issue Development Permit No. DP1173 at 1421 Princess Royal Avenue with the following variances:

- reduce the minimum front yard setback from 6m to 1.5m;
- reduce the minimum north side yard setback from 3m to 0.45m;
- reduce the minimum rear yard setback from 10.5m to 7.35m;
- reduce the minimum front yard setback for an underground parking structure from 1.8m to 1.5m;
- increase the maximum allowable combined height of a fence and retaining wall in a side and rear yard from 2.4m to 3.3m; and
- increase the maximum allowable percentage of small car parking from 40% to 50%. |

## BACKGROUND

A development permit application, DP1173, was received from Raymond De Beeld Architect Inc., on behalf of Roche Lookout Holdings Inc., in order to permit a new multi-family residential project at 1421 Princess Royal Avenue.

### **Subject Property and Site Context**

<i>Zoning</i>	R8 – Medium Density Residential
<i>Location</i>	The subject property is located on the west side of Princess Royal Avenue, adjacent to a lane, between Larch Street and Estevan Road.
<i>Total Area</i>	911m <sup>2</sup>
<i>Official Community Plan (OCP)</i>	Map 1 – Future Land Use Plans – Corridor Map 3 – Development Permit Area – DPA No. 9 – Commercial, Industrial, Institutional, Multiple Family and Mixed Commercial/Residential Development.
<i>Neighbourhood Plan</i>	Newcastle + Brechin Neighbourhood Plan – Mixed Use Corridor
<i>Relevant Design Guidelines</i>	General Development Permit Area Design Guidelines; Newcastle + Brechin Neighbourhood Plan Urban Design Framework and Guidelines

The subject property consists of two existing lots and is located in the Brechin Hill Community. The property slopes significantly downhill from southwest to northeast and provides views towards Departure Bay and Newcastle Channel. There is currently a small single residential dwelling on the property.

The surrounding neighbourhood includes older three- to four-storey multi-family residential buildings to the west and south, and single residential dwellings to the north and east. Terminal Park shopping plaza is located across Estevan Road, 60m west of the subject property.

## **DISCUSSION**

### **Proposed Development**

The applicant is proposing a four-storey infill multi-family residential building with 16 dwelling units and under-the-building parking. The parking level will be mostly underground, but will be exposed above grade at the lowest portion of the subject property. As such, the building will present a five-storey elevation at its northeast corner. The proposed unit composition will include 12 one-bedroom units and 4 two-bedroom units.

The proposed building will have a Floor Area Ratio (FAR) of 1.56. The R8 zone permits a base density (FAR) of 1.25. An additional 0.21 of FAR is permitted for the portion of parking that is underground and an additional 0.1 of FAR is permitted for meeting the Tier 1 requirements as described in the “City of Nanaimo Zoning Bylaw 2011 No. 4500” ‘Schedule D – Amenity Requirements for Additional Density’. The applicant is proposing to meet Tier 1 by providing amenities, including the following:

- scooter parking;
- exceeding the BC Energy Step Code requirement by one step;
- a green roof; and
- educational signage regarding sustainable transportation, energy management, and water management.

### *Site Design*

As a smaller rectangular lot, the development’s siting is dictated by sloping topography and vehicle entry points. Vehicle access to the site is proposed from the lane to the north, as required by the City of Nanaimo “Off-Street Parking Regulations Bylaw 2018 No. 7266”. A portion of the lane will be widened between the vehicle entry and Princess Royal Avenue, and protected by a Statutory Right-of-Way to facility two-way traffic on the lane to the property.

All required vehicle parking and long-term bicycle parking will be provided in the under-the-building parking level. A bike rack for short-term bicycle parking will be installed outside the primary entrance. Garbage and recycling pick-up will be in the parking level.

The primary entrance will be on the south side of the building, accessed via a walkway and entry court from Princess Royal Avenue at the southeast corner of the site. At this location, the grade of the sidewalk will meet the grade of the ground floor for the building. A private walkway will continue around the south and west of the building to connect with the laneway to the north. A common outdoor amenity space is proposed on the west side of the building, in addition to the

individual patios for ground-level units. A secondary entrance at the northeast corner of the site will provide pedestrian access to the parking level and secure bicycle storage.

In order to accommodate fire vehicle access within 15m of the primary building entrance, the applicant will be carrying out frontage works and services in front of both the subject property and the property to the south.

#### *Building Design:*

The building design reflects the guidelines for massing, form, and character as expressed in the Newcastle + Brechin Neighbourhood Plan Urban Design Framework and Guidelines. The site is below the minimum lot area for new R8-zoned lots, and as such, a number of variances are requested to accommodate the compact, infill building form. The building is sited to maximize views to the north, and the massing of the building keeps in context with adjacent multi-family residential buildings at 1430 and 1420 Estevan Road, to the west and south respectively. The proposed building is a modern interpretation of the neighbouring flat-roof apartment buildings and is lower in peak elevation than either of these buildings.

A simple roofline and striking geometric protruding windows are provided on the west and east elevations. The south elevation will have less articulation, but will include off-set coloured windows and vertical windows above the main lobby. The north elevation will feature large covered balconies for all units on the second to fourth storeys, separated by vibrantly coloured glass screens. Ground-level units will have large patios. The primary cladding on the west, south, and east elevations is vertical fibre cement panels. The exposed exterior of the parking level is proposed to be concrete, with stone cladding to highlight the vehicle entrance.

The internal layout of the building will have common areas, including lobby, elevator, stairs, and storage concentrated on the south side of the building, with all units facing north towards Departure Bay.

#### *Landscape Design*

A planter containing boxwood and groundcovers is proposed along the north property line, with vines planted at the base of the parking level wall. A planted boulevard with street trees is proposed along Princess Royal Avenue. Medium evergreen groundcovers in planters will be located next to the entry court and entrance walkway along the south property line. The common outdoor amenity space on the west side of the property will be enclosed and will include a lawn, patio, planters, and small deciduous trees. A grass-and-meadow-mix green roof is proposed on the building, as well as on the roof of the parking level vehicle entrance.

#### **Design Advisory Panel**

The Design Advisory Panel (DAP), at its meeting held on 2020-FEB-27, accepted DP1173 as presented and provided the following recommendation:

- Consider ways to enhance the entranceway on the east elevation.

In response to the DAP recommendation, the applicant revised the development plans by proposing wayfinding signage on the east elevation on a covered canopy structure.

## Proposed Variances

### *Minimum Front Yard Setback*

The minimum front yard setback in the R8 zone is 6.0m. The proposed front yard setback for a portion of the under-the-building parking level is 1.5m; a requested variance of 4.5m.

The parking level will be primarily underground on the front (east) elevation, but a portion will be exposed above grade closer to north property line adjacent to the lane. The exposed portion of the parking level will include a landscape buffer and will also feature window wells with coloured glass. The remainder of the building above the parking level will be set back 4.5m from the front property line.

### *Minimum Side Yard Setback*

The minimum side yard setback in the R8 zone is 3.00m. The proposed north side yard setback for a portion of the under-the-building parking level is 0.45m; a requested variance of 2.55m.

The building will primarily have a setback of 8.3m from the north side property line; however, a portion of the underground parking level will be exposed above grade 0.45m away from the property line. The parking level is proposed to encroach into the north setback in order to accommodate required parking stall depths and drive aisle widths. An alternative option, to raise the grade surrounding the parking level with retaining walls, would have a greater impact and was discouraged. Vines will be planted to screen the exposed parking level wall on the north elevation, and coloured glass windows are proposed nearest to the vehicle entrance.

The adjacent lane is not anticipated to be adversely impacted by the setback variance, and the lane will be improved to be functional for two-way traffic between the development and Princess Royal Avenue.

### *Minimum Rear Yard Setback*

The minimum rear yard setback in the R8 zone is 10.50m. The proposed rear yard setback is 7.35m, a requested variance of 3.15m.

The rear yard setback variance is requested to accommodate the apartment building form on a smaller R8 lot. The finished grade of the development will be approximately 3m below grade of the neighbouring uphill properties. As such, the building and its encroachment into the rear yard will have less of a visual impact.

### *Projections Into Yards*

The minimum front yard setback for an underground parking structure is 1.8m. The proposed parking structure setback from Princess Royal Avenue is 1.5m; a requested variance of 0.3m.

The parking setback variance is proposed in order to accommodate the required number of parking stalls.

### *Maximum Fence Height*

The maximum allowable fence and retaining wall height in a side or rear yard in the R8 zone is 2.4m. A proposed fence and retaining wall will have a combined a height of 3.3m; a requested variance of 0.9m.

To provide a functional development area, the applicant is proposing to cut into the existing grade and lower the elevation onsite. This will necessitate perimeter retaining walls on the south (side) and west (rear) property lines. The smooth concrete retaining wall will face internally and will be screened by vegetation as part of the common amenity area. A small glass guardrail is proposed on top of the retaining walls for safety.

### *Small Car Parking*

The maximum allowable amount of small car parking spaces is 40% of the required parking spaces (22 spaces). The applicant is proposing 50% of the required parking (11 spaces) to be small car parking; a requested variance of 10% (2 spaces).

The small car parking variance is requested in order to accommodate the required parking. Due to the constrained nature of the lot, providing an additional two full-size vehicle parking spaces would not be feasible.

Staff support the proposed variances.

### **SUMMARY POINTS**

- Development Permit Application No. DP1173 is for a four-storey infill multi-family residential building with 16 dwelling units and under-the-building parking.
- Variances are requested to reduce the minimum front, rear, and side yard setbacks; to increase the maximum allowable combined height of a fence and retaining wall in a side and rear yard; and to increase the maximum allowable percentage of small car parking.
- Staff support the proposed variances.

## **ATTACHMENTS**

ATTACHMENT A: Permit Terms and Conditions  
ATTACHMENT B: Context Map  
ATTACHMENT C: Location Plan  
ATTACHMENT D: Site and Parking Plans  
ATTACHMENT E: Building and Retaining Wall Elevations  
ATTACHMENT F: Building Renderings  
ATTACHMENT G: Landscape Plan and Details  
ATTACHMENT H: Schedule D – Amenity Requirements for Additional Density  
ATTACHMENT I: Aerial Photo |

### **Submitted by:**

Lainya Rowett  
Manager, Current Planning |

### **Concurrence by:**

Jeremy Holm  
Director, Development Approvals

Dale Lindsay  
General Manager, Development Services|

# ATTACHMENT A PERMIT TERMS AND CONDITIONS

## TERMS OF PERMIT

The “City of Nanaimo Zoning Bylaw 2011 No. 4500”, is varied as follows:

1. *Section 6.5.1 Projections Into Yards* – to reduce the minimum front yard setback for an underground parking structure from 1.8m to 1.5m.
2. *Section 6.10.2 Fence Height* – to increase the maximum combined height of a fence and retaining wall within a side and rear yard from 2.4m to 3.3m.
3. *Section 7.5.1 Siting of Buildings* – to reduce the minimum required front yard setback from 6.0m to 1.5m.
4. *Section 7.5.1 Siting of Buildings* – to reduce the minimum north side yard setback from 3.00m to 0.45m for the parking level of the building.
5. *Section 7.5.1 Siting of Buildings* – to reduce the minimum required rear yard setback from 10.50m to 7.35m.

The City of Nanaimo “Off-Street Parking Regulations Bylaw 2018 No. 7266” is varied as follows:

1. *Section 4.3 Small Car Spaces* – to increase the permitted percentage of small car parking spaces from 40% to 50%.

## CONDITIONS OF PERMIT

1. The subject property shall be developed generally in accordance with the proposed Site and Parking Plans prepared by Raymond de Beeld Architect Inc., dated 2020-NOV-16 and 2021-JAN-15, as shown on Attachment D.
2. The development is in substantial compliance with the proposed Building and Retaining Wall Elevations prepared by Raymond de Beeld Architect Inc., dated 2020-OCT-23 and 2021-JAN-15, as shown on Attachment E.
3. The subject property is developed in substantial compliance with the proposed Landscape Plan and Details prepared by Kate Stefiuk Studio, dated 2020-JUL-10, as shown on Attachment G.
4. The subject property is developed in accordance with the ‘Schedule D – Amenity Requirements for Additional Density’ prepared by Raymond de Beeld Architect Inc, received 2020-JUL-17, as shown in Attachment H, and is to include the following items:
  - A letter from the coordinating professional submitted prior to Building Permit issuance outlining how the required items for additional density will be achieved; and
  - A letter from the coordinating professional with accompanying evidence submitted prior to building occupancy demonstrating that the required items have been provided.

PERMIT TERMS AND CONDITIONS  
PAGE 2

5. The subject site is to be consolidated into a single property prior to Building Permit issuance.
6. A Statutory Right-of-Way for public vehicle access is to be registered over the portion of the property identified on Attachment D prior to building occupancy.
7. Frontage works & services to the south of the property as identified on Attachment D are completed prior to building occupancy, to the satisfaction of the Director of Development Approvals.

# ATTACHMENT B CONTEXT MAP

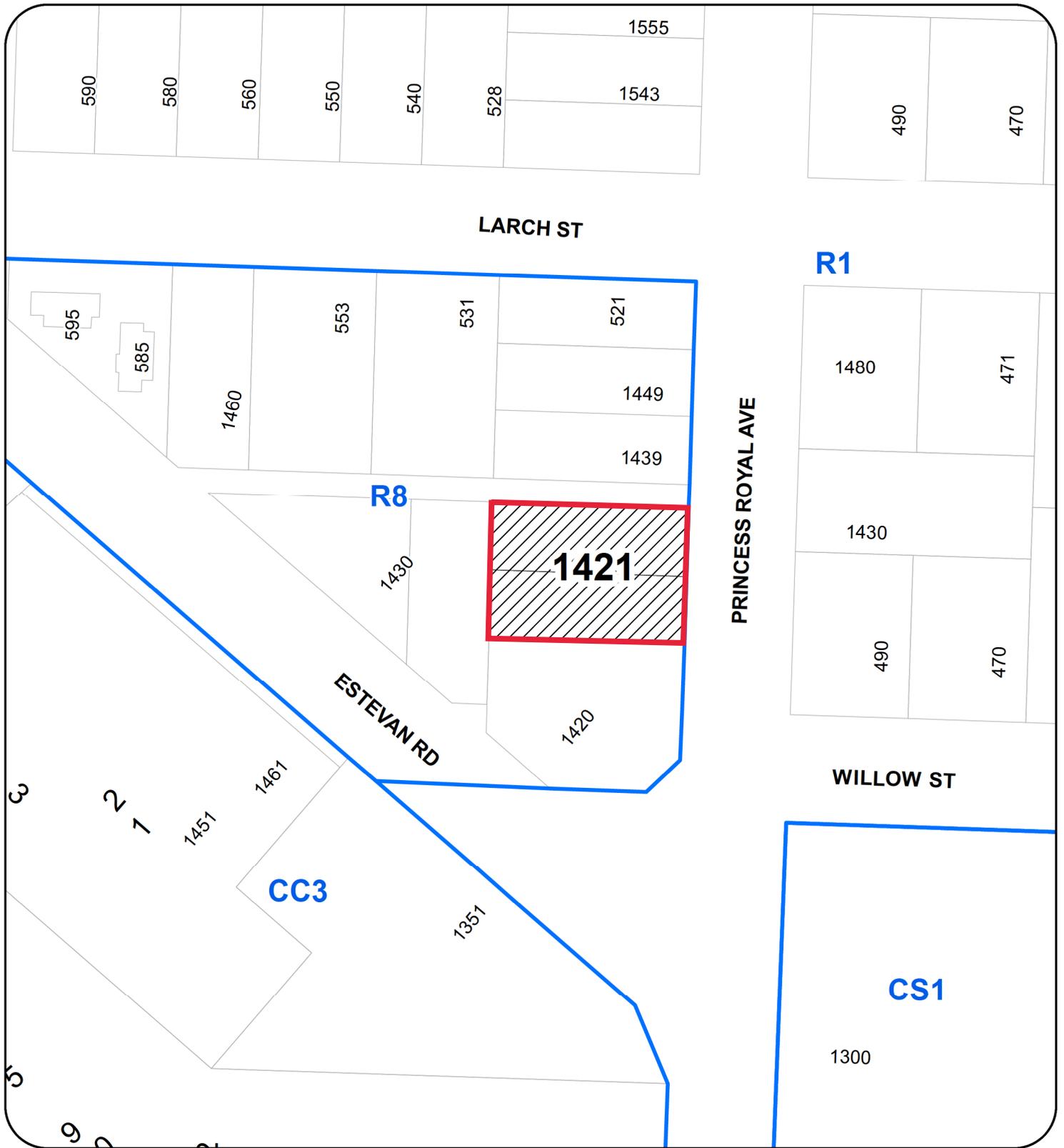


DEVELOPMENT PERMIT APPLICATION NO. DP1173 - 1421 PRINCESS ROYAL AVENUE



1421 Princess Royal

# ATTACHMENT C LOCATION PLAN



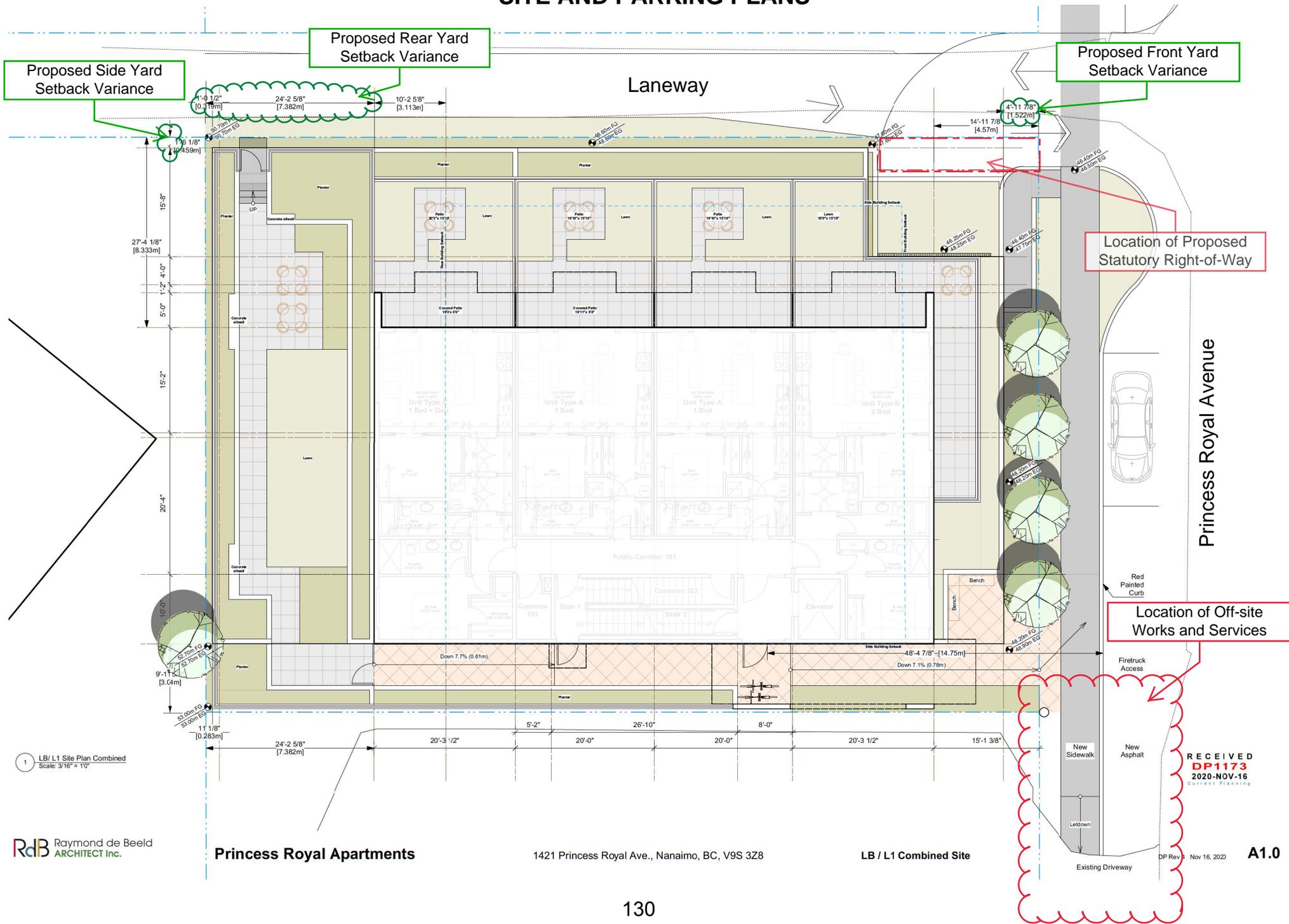
## DEVELOPMENT PERMIT APPLICATION NO. DP001173 LOCATION PLAN



SUBJECT PROPERTY

Civic: 1421 PRINCESS ROYAL AVENUE  
Legal: LOT 2, BLOCK 8, NEWCASTLE RESERVE, SECTION 1  
NANAIMO DISTRICT, PLAN 1972 (P.I.D. 004-598-890) LOT 3  
BLOCK 8, NEWCASTLE RESERVE, SECTION 1  
NANAIMO DISTRICT, PLAN 1972 (P.I.D. 004-598-954)

# ATTACHMENT D SITE AND PARKING PLANS



1 LB/ L1 Site Plan Combined  
Scale: 3/16" = 1'

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# ATTACHMENT E BUILDING and RETAINING WALL ELEVATIONS



North



East

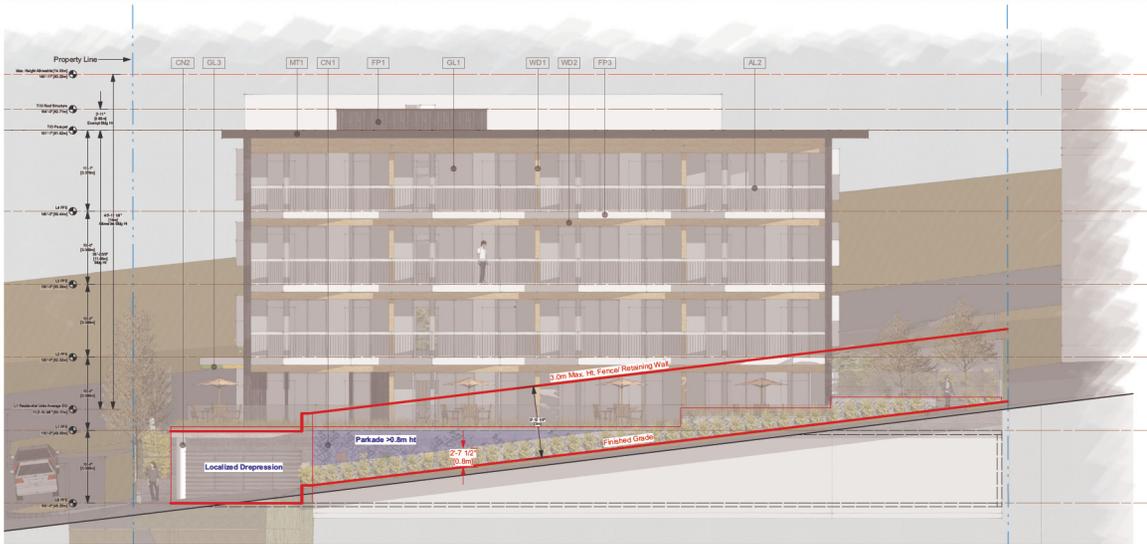


South

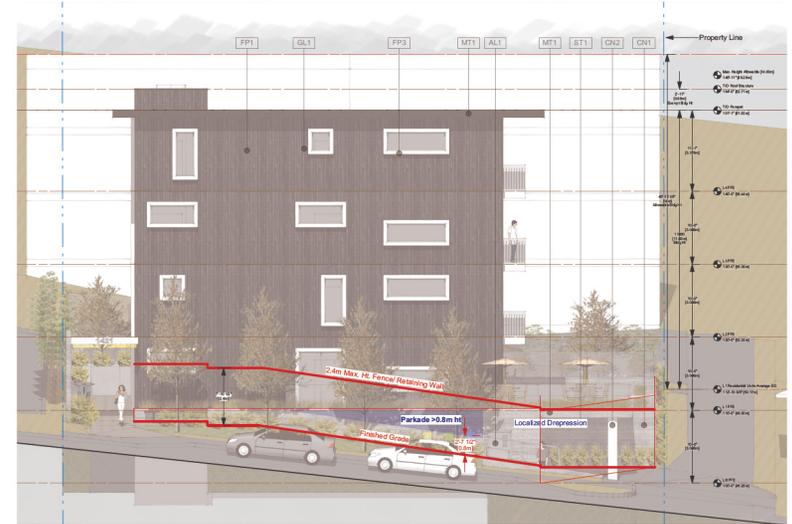


West

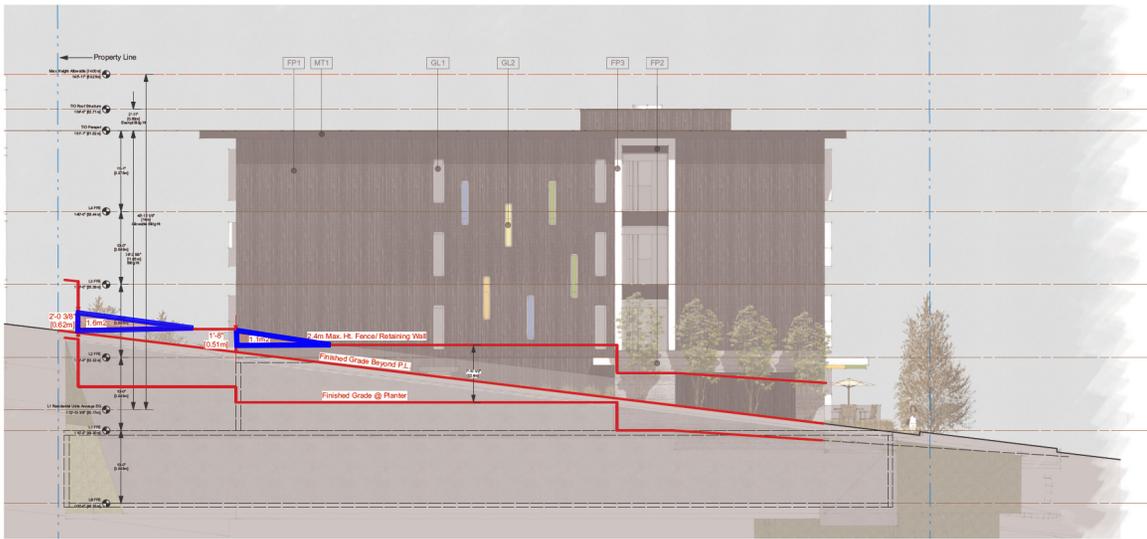
- MATERIAL LEGEND:**
- AL1 - Aluminum Door System - Wood Chad
  - AL2 - Aluminum Recessed - White
  - C1 - Concrete Wall - Natural Stain
  - C2 - Concrete Column - Natural Stain
  - FP1 - Fibre Cement Panel - Dark Grey (Vertical)
  - FP2 - Fibre Cement Panel - Black
  - FP3 - Fibre Cement Panel - White
  - GL1 - Vinyl Window System
  - GL2 - Vinyl Window System - Coloured Glass
  - GL3 - Glass Fin - Coloured
  - MT1 - Metal Flashing - Black
  - ST1 - Stone Cladding - Brown
  - WD1 - Cedar Natural Column
  - WD2 - Cedar Natural Beam



1 NORTH  
Scale: 1/8" = 1'-0"



2 EAST  
Scale: 1/8" = 1'-0"



3 SOUTH  
Scale: 1/8" = 1'-0"



4 WEST  
Scale: 1/8" = 1'-0"

# ATTACHMENT F BUILDING RENDERINGS



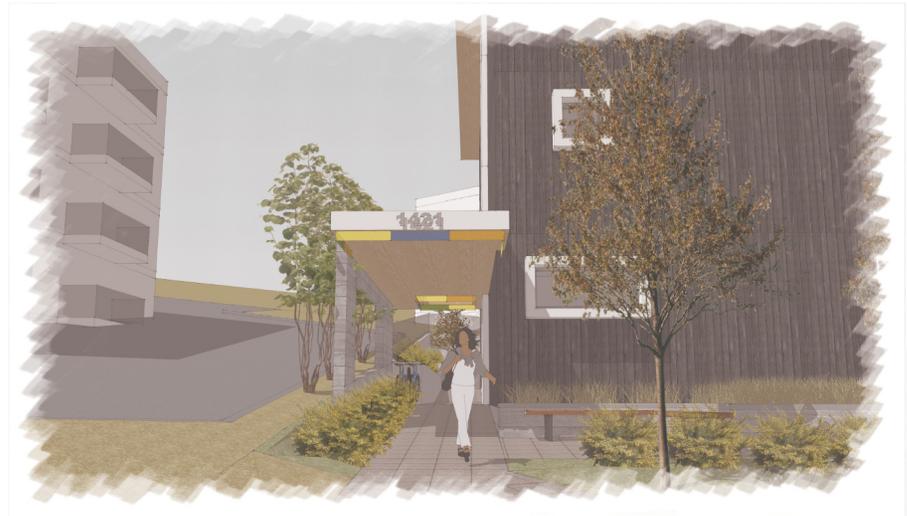
1 NE Perspective



2 NW Perspective



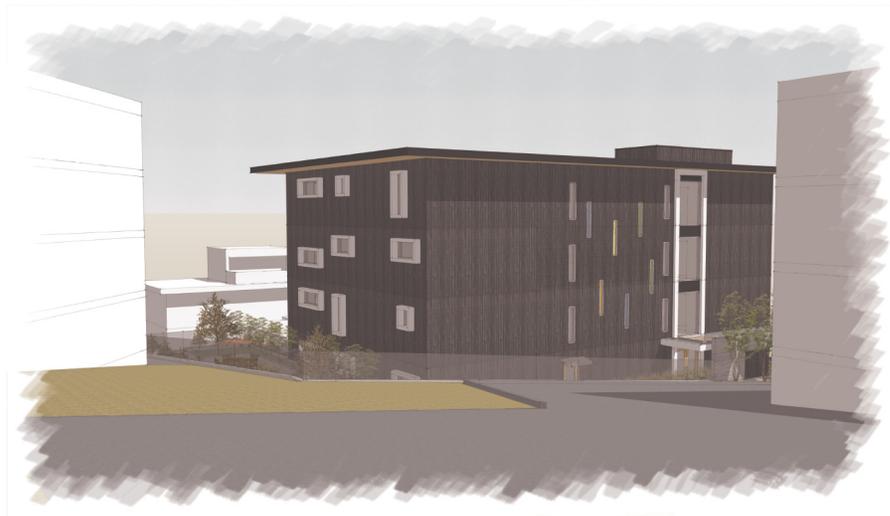
3 Main Entry (from Princess Royal Ave.)



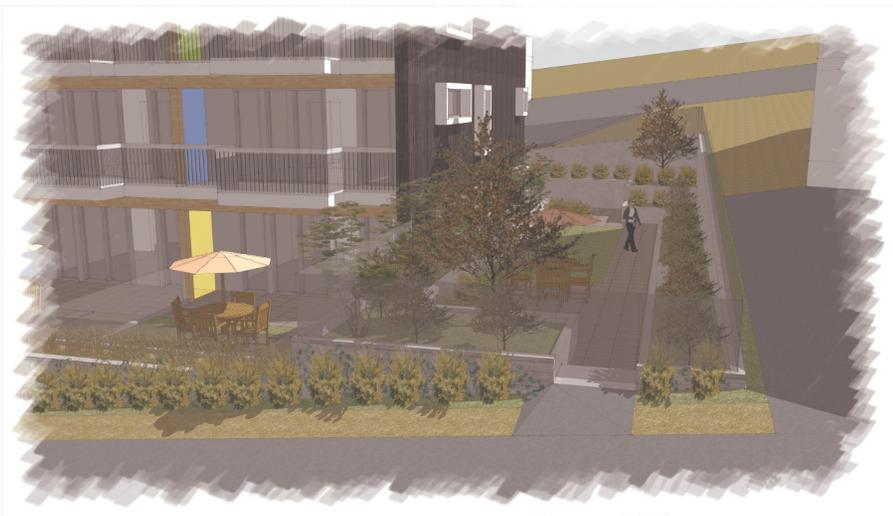
4 Main Entry (Close)



1 NE Perspective from Princess Royal Ave.



2 SW Corner

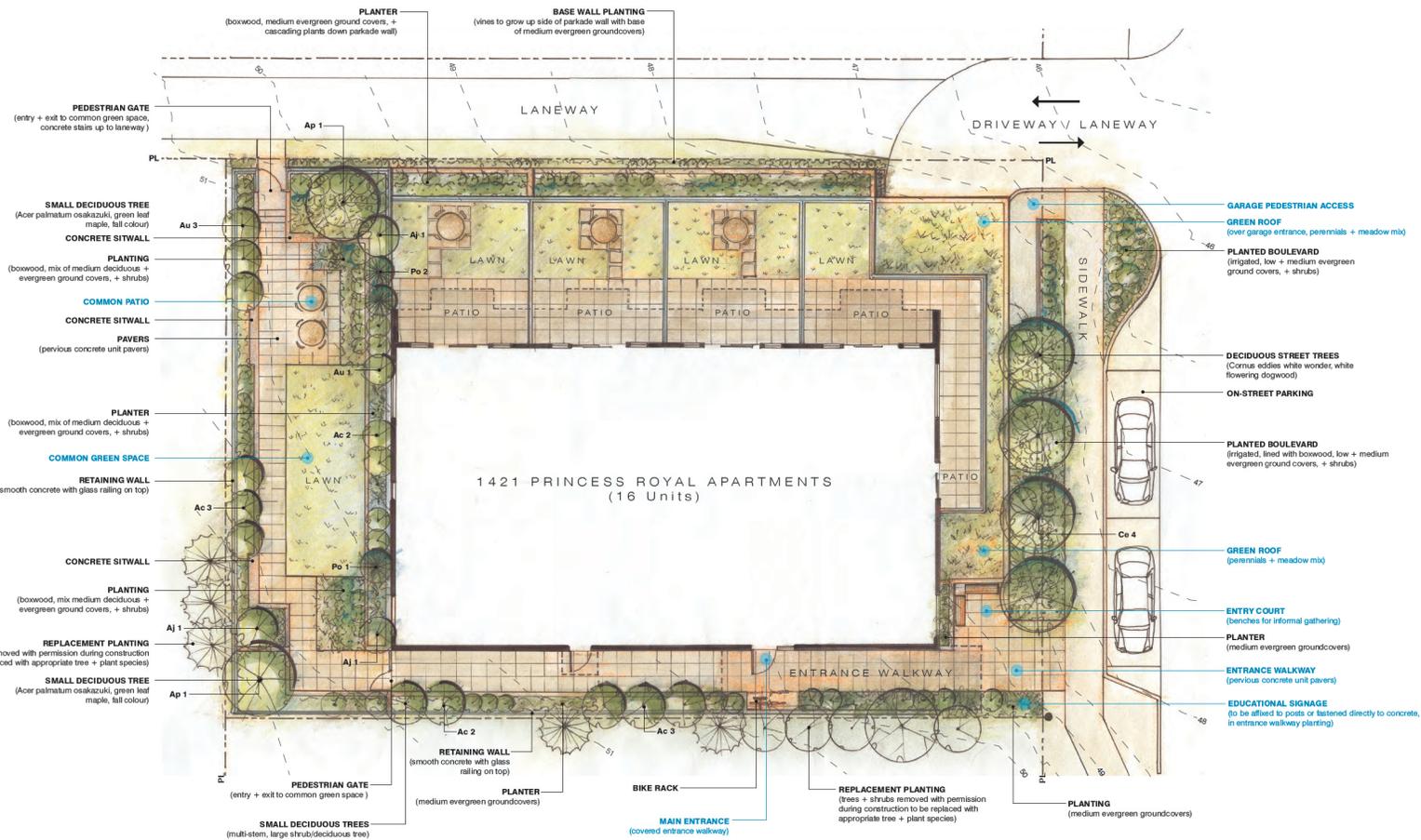


3 NW Perspective from Lane



4 Podium - Inside Corner of Lot

# ATTACHMENT G LANDSCAPE PLAN AND DETAILS



**LANDSCAPE PLAN**  
SCALE 1:100



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2020-JUL-17  
COURT PLANNING

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**CLIENT**

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rony@ronyhomes.com

NO.	DATE	ISSUE
1	12-09-19	DP SUBMISSION

NO.	DATE	REVISION
1	07-10-20	DP REVISION

**PROJECT**

**PRINCESS ROYAL APTS**  
1421 Princess Royal Avenue  
Nanaimo, BC

**LANDSCAPE PLAN**

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PROJECT	NO.	DATE
DB	KS	19005
CB	KS	

SCALE	DATE
1:100	December 4, 2019

**L1.02**

## DESIGN PRECEDENTS



01 White Flowering Dogwood



02 Small Deciduous Trees (Japanese Maple)



03 Evergreen Groundcovers



04 Evergreen Shrubs (Arbutus unedo)



05 Boxwood with Mixed Planting



06 Green Roof



07 Entry Court Benches



08 Pathway Lighting (recessed wall lights) Entrance & Common Green Space



09 Entrance Walkway



10 Educational Signage

## DESIGN RATIONALE

### CONTEXT

The parcel at 1421 Princess Royal Avenue lies beyond the portion of the street intended to function as the High Street for the neighbourhood. The block is designated for higher density residential infill. The proposed development is a 16-unit residential building situated in a landscape that responds directly to the *Newcastle + Brechin Neighbourhood Plan* by strengthening the human scale, improving neighbourhood character and walkability, and enhancing the ecological function of the urban environment.

### DESIGN CONCEPT

The design concept for 1421 Process Royal Avenue is to elevate the relationships between people, place and planting into a formal, functional urban ecosystem.

- The planting plan is structured around a base of indigenous species.
- Complementary drought-tolerant ornamentals add character and integrate ecosystem function and urban form.
- Plantings utilize vertical layering to create a lush landscape with visual interest, structural diversity and year round function.

### Other key landscape features include:

- Sidewalks to provide space for pedestrians and contribute to neighbourhood walkability;
- Street trees to provide screening and shading for the building, strengthen the pedestrian experience and contribute to rainwater management and habitat value.
- An accessible entry court to provide a semi-private seating area between the building and the sidewalk, creating a more welcoming, human-scaled environment.
- Curb bulges and permeable hardscaping to allow for rainwater infiltration.
- A green roof feature over the parking entry to further unify the architecture with the local ecology.
- A common green space at the rear of the building to offer a garden refuge and gathering space for residents.
- Educational signage to highlight key features of the development + landscape.

### EDUCATIONAL SIGNAGE

#### Sustainable Transportation

The educational signage for sustainable transportation will show a 400m radius circle centred on the Princess Royal Apartments to illustrate the amenities, active transportation linkages and transit routes within a 5-minute walk.

Graphics and text will highlight the importance of transportation options in a sustainable city, the walkability of the Brechin-Newcastle Neighbourhood, and its connections to the larger City, as well as the inclusion of charging stations for electric vehicles, ebikes and scooters in the development.

#### Sustainable Water Management

The educational signage for sustainable water management will feature the landscape plan for 1421 Princess Royal Ave., identifying key elements that capture and slow rainwater and reduce landscape water demand.

Graphics and text will highlight how green roofs, deep soils, and drought tolerant and indigenous plants create healthy and functional urban ecosystems, temperatures and longer droughts.

## PLANT PALETTE

Key	Botanical Name	Common Name
<b>Deciduous Trees</b>		
Ap	Acer palmatum Osakazuki	Japanese Maple
Aj	Acer japonicum Shindeshojo	Japanese Maple
Ac	Acer circinatum	Vine Maple
Ce	Cornus eddies white wonder	Eddies White Wonder Dogwood
<b>Coniferous Trees</b>		
Po	Picea omarika bruns	
<b>Evergreen Shrubs</b>		
Au	Arbutus unedo	Strawberry Bush
Bs	Buxus sempervirens	Boxwood
Gs	Gaultheria shallon	Salal
Mn	Mahonia nervosa	Dull Oregon Grape
R	Rhododendron 'Glacier'	Evergreen Azalea
Vo	Vaccinium ovatum	Evergreen Huckleberry
<b>Deciduous Shrubs</b>		
Rs	Ribes sanguineum	Red Flowering Currant
V	Vaccinium (various)	Blueberry
<b>Ground Cover</b>		
Au	Arcostaphylos uva-ursi	Kinnikinnick
Es	Epimedium spheerium	Epimedium
Fc	Fragaria chinkensis	Coastal Strawberry
Sr	Sedum rupestre 'Angelina'	Angelina Stonecrop
<b>Ferns</b>		
Bs	Blechnum spicant	Deer Fern
Pg	Polypodium glycyrrhiza	Licorice Fern
Pm	Polystichum munitum	Sword Fern
<b>Grasses</b>		
Hm	Hakonechloa macra	Japanese Forest Grass
Sc	Sesleria caerulea	Blue Moor Grass
<b>Vines</b>		
Pt	Parthenocissus tricuspidata	Boston Ivy
<b>Green Roof</b>		
Am	Armeria maritima	Sea Thrift
Ca	Cerastium arvense	Field Chickweed
EI	Eriophyllum lanatum var. lanatum	Woolly Sunflower
Fr	Festuca roemerii	Roemer's Fescue
Sra	Sedum rupestre Angelina	Angelina Stonecrop
Th	Triteleia hyadnthia	Fools Onion
<b>Seeds</b>		
	Pickseed Coastal Native Sodgrass Mix or equivalent	75% Shoreline Creeping Slender Fescue 17% Canby Canada Bluegrass 8% Schoen Slender Hairgrass

### KATE STEFIUK STUDIO

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### CLIENT

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tory@rochyhills.com

### NO. | DATE | ISSUE

1 | 12-09-19 | DFC SUBMISSION

### NO. | DATE | REVISION

1 | 07-10-20 | DFC REVISION

### PROJECT

#### PRINCESS ROYAL APTS

1421 Princess Royal Avenue  
Nanaimo, BC

### LANDSCAPE PLAN

DESIGN RATIONALE  
DESIGN PRECEDENTS  
PLANT PALETTE

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DB KS 19005 CB KS

SCALE 1:100  
DATE December 4, 2019

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2020-JUL-17  
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## ATTACHMENT H

### SCHEDULE D - AMENITY REQUIREMENTS FOR ADDITIONAL DENSITY

Category 3: Parking and Sustainable Transportation (10 points required)

Amenity		Points
A	Long term protected bicycle storage is provided and shower and change room facilities are provided to accommodate building employees where applicable.	3
B	At least one parking space is clearly marked and designated for the exclusive use of a vehicle belonging to a car share or car co-op.	1
C	The developer purchases a new car and gifts the car to a recognized car share provider for the inclusion of a car share space on the subject property.	4
D	The parking area within the proposed development includes at least one electric vehicle charging station.	①
E	A minimum of 80% of the total parking area is located underground or in a parking structure incorporated into the design of the building.	④
F	The proposed development includes covered and designated parking spaces for a motorized scooter or plug-in for an electronic bicycle or electric scooter, or a designated motorcycle parking space to accommodate the following number of spaces: <ul style="list-style-type: none"> <li>a) multiple family residential developments: 1 motorized scooter or motorcycle space per 15 dwelling units; and</li> <li>b) non-residential uses: 1 motorized scooter or motorcycle space per 600m<sup>2</sup> of Gross Floor Area for the first 5000m<sup>2</sup> plus one space per 1500m<sup>2</sup> of additional Gross Floor Area.; and</li> <li>a) a minimum of one electronic plug-in is provided to accommodate at least one electric scooter or electronic bicycle.</li> </ul>	②
G	A pedestrian network is included in the proposed development that connects the buildings on the site with the public road right-of-way and, the pedestrian network from the adjacent site to which there is access by perpetual easement or right-of-way, provided the City agrees to accept the right-of-way.	2
H	Parking does not exceed minimum parking requirements within the City's Development Parking Regulations Bylaw.	②
I	The development includes signage or display(s) regarding sustainable transportation alternatives available on site or within the immediate area.	①
Total		10 20

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2020-JUL-17  
Current Planning

## Category 5: Energy Management (11 points required)

Amenity		Points
A	The proposed development meets at least the requirements of Step 2 of the BC Energy Step Code and exceeds the requirement specified in the Building Bylaw by one step.*	10
B	The proposed development meets at least the requirements of Step 3 of the BC Energy Step Code and exceeds the requirement specified in the Building Bylaw by two steps.*	15
C	The proposed development is considered a Part 3 within the British Columbia Building Code (BCBC) and the building meets the minimum requirements of Step 4 (Net Zero Ready) within the BC Energy Step Code; or The proposed development is considered a Part 9 within the British Columbia Building Code (BCBC) and the building meets the minimum requirements of Step 4 or 5 within the BC Energy Step Code.*	30
D	The development includes permanent education signage or display(s) regarding sustainable energy management practices used onsite.	1
Total		11 56

\* Points will be awarded for only one of A, B, or C.

## Category 6: Water Management (8 points required)

Amenity		Points
A	At least 50% of the property is covered with a permeable surface area which may include a green roof.	2
B	The proposed buildings on the property include plumbing features which will use 35% less water than the BC Building Code standard.	2
C	A green roof is installed to a minimum 30% of the roof area.	3
D	A living wall is installed to cover at least 10% of the total available wall area for the proposed project.	2
E	A non-potable irrigation system is installed and used for all on-site irrigation.	3
F	A water efficient irrigation system (such as drip) is installed.	1
G	The proposed development includes a rain garden, cistern, bioswale or storm water retention pond on the property.	2
H	The development site includes permanent educational signage or a display(s) regarding sustainable water management practices used on site.	1
Total		9 16

**ATTACHMENT I  
AERIAL PHOTO**



N



**DEVELOPMENT PERMIT APPLICATION NO. DP001173**

**Legend**



SUBJECT PROPERTY